

# Agreement in Principle for Highway-related Departures

## DEPARTURES FORM

*The Project Sponsor makes the following departure submission for consent / approval by the Principal Project Sponsor and the Assistant Director for Highways and Transport:*

| General Details                                     |  |
|---|--|
| Project Name  | DIDCOT TO CULHAM RIVER CROSSING,<br>DIDCOT GARDEN TOWN, HIF1 |
| OCC Project Sponsor <i>(name &amp; team)</i>        | Aron Wisdom, Major Projects Development                      |
| Lead Designer details <i>(name &amp; job title)</i> | David Riach, Associate, AECOM                                |
| Highway authority name                              | Oxfordshire County Council                                   |
| Date of submission and version number               | 19 January 2021, RIV-DS-07 version P02                       |

| Project Details  |   |
|--|---|
| Road number <i>(where applicable)</i>                            | N/A   |
| Road name(s) <i>(where applicable)</i>                           | This route is new. Road name to be decided.   |
| Location <i>(nearest town/ city)</i>                             | Didcot  |
| Road category and type <i>(where applicable)</i>                 | A road, S2 all-purpose single carriageway   |
| National Speed limit <i>(where applicable)</i>                   | Proposed to be 50mph (85kph design speed)   |
| Summary description of existing conditions and key design issues | <p>No existing conditions as this is a proposed road which will be newly constructed.</p> <p>The key design issue is the requirement of an overtaking value of 30% for rural S2 roads. Taking the rural extent of the link road as 3.3km (out of its overall 3.6km length), 30% equates to approximately 1.0km per direction.</p> <p>Due to the constraints along the proposed road, the required 30% overtaking value cannot be achieved. These constraints include the number and proximity of junctions and other features along the route, two bridge structures and flood plain. The proposed design provides an overtaking value of 26%.</p> <p>As a result, 1 no. Departure from Standard is required.</p> |

| Departure Details                                  |   |                     |                         |              |            |  |  |            |  |  |            |  |  |                |              |            |                |              |            |     |     |     |     |     |     |      |      |     |      |      |     |      |      |     |      |      |     |       |  |     |       |  |     |                         |  |       |                         |  |       |
|--|---|---------------------|-------------------------|--------------|------------|--|--|------------|--|--|------------|--|--|----------------|--------------|------------|----------------|--------------|------------|-----|-----|-----|-----|-----|-----|------|------|-----|------|------|-----|------|------|-----|------|------|-----|-------|--|-----|-------|--|-----|-------------------------|--|-------|-------------------------|--|-------|
| Discipline   | Highways  |                     |                         |              |            |  |  |            |  |  |            |  |  |                |              |            |                |              |            |     |     |     |     |     |     |      |      |     |      |      |     |      |      |     |      |      |     |       |  |     |       |  |     |                         |  |       |                         |  |       |
| Type   | Geometric   |                     |                         |              |            |  |  |            |  |  |            |  |  |                |              |            |                |              |            |     |     |     |     |     |     |      |      |     |      |      |     |      |      |     |      |      |     |       |  |     |       |  |     |                         |  |       |                         |  |       |
| Relevant technical standards                       | DMRB CD 109 Revision 1 - March 2020, Highway Link Design  |                     |                         |              |            |  |  |            |  |  |            |  |  |                |              |            |                |              |            |     |     |     |     |     |     |      |      |     |      |      |     |      |      |     |      |      |     |       |  |     |       |  |     |                         |  |       |                         |  |       |
| Underlying standard <i>(if applicable)</i>         | DMRB  |                     |                         |              |            |  |  |            |  |  |            |  |  |                |              |            |                |              |            |     |     |     |     |     |     |      |      |     |      |      |     |      |      |     |      |      |     |       |  |     |       |  |     |                         |  |       |                         |  |       |
| Difference between standard(s) and proposed design | <p>RIV-DS-07</p> <p>CD 109 Paragraph 9.2 states "The minimum overtaking value for rural S2 roads shall be 30%."</p> <p>The proposed design provides an overtaking value of 26% northbound and 27% southbound.</p>   |                     |                         |              |            |  |  |            |  |  |            |  |  |                |              |            |                |              |            |     |     |     |     |     |     |      |      |     |      |      |     |      |      |     |      |      |     |       |  |     |       |  |     |                         |  |       |                         |  |       |
| Reason for departure                               | <p>The required 30% overtaking value cannot be achieved due to the constraints to the proposed road geometry. Considering the rural element of the road as being the 3.27km extent from immediately north of the raised parallel crossing at Ch. 355 to Ch. 3625 at Abingdon Roundabout, the following constraints are present:</p> <ul style="list-style-type: none"><li>- Two roundabout junctions (one incorporating a segregated left turn lane (SLTL))</li><li>- Two ghost island priority junctions</li><li>- Three direct accesses</li><li>- Two bus laybys (one northbound and one southbound)</li><li>- Two controlled pedestrian/cyclist crossing points (one with associated equestrian crossing)</li><li>- One uncontrolled pedestrian/cyclist crossing point</li><li>- Two non-overtaking horizontal curves</li><li>- Four non-overtaking crest curves (vertical alignment)</li><li>- Two bridge structures, one over railway sidings and one over the river and associated flood plain.</li></ul> <p>The proposed horizontal alignment has been kept as straight as possible to maximise the overtaking opportunity with level overtaking sections. The constraints noted above also limit the provision of alternative overtaking options such as climbing lanes, dual carriageway overtaking sections or wide single 2+1 overtaking lanes.</p> <p>The proposed design (version P04) provides an overtaking value of 26% northbound and 27% southbound. The sections are listed in Table 1 below.</p> <p>Table 1 Overtaking Sections</p> <table><tr><th colspan="6">Overtaking Sections</th></tr><tr><th colspan="3">Northbound</th><th colspan="3">Southbound</th></tr><tr><th>Start Chainage</th><th>End Chainage</th><th>Length (m)</th><th>Start Chainage</th><th>End Chainage</th><th>Length (m)</th></tr><tr><td>355</td><td>510</td><td>155</td><td>575</td><td>430</td><td>145</td></tr><tr><td>2340</td><td>2485</td><td>145</td><td>2740</td><td>2420</td><td>320</td></tr><tr><td>2620</td><td>3170</td><td>550</td><td>2840</td><td>3260</td><td>420</td></tr><tr><td colspan="2">Total</td><td>850</td><td colspan="2">Total</td><td>885</td></tr><tr><td colspan="2">%age of 3270 rural link</td><td>26.0%</td><td colspan="2">%age of 3270 rural link</td><td>27.1%</td></tr></table> | Overtaking Sections |                         |              |            |  |  | Northbound |  |  | Southbound |  |  | Start Chainage | End Chainage | Length (m) | Start Chainage | End Chainage | Length (m) | 355 | 510 | 155 | 575 | 430 | 145 | 2340 | 2485 | 145 | 2740 | 2420 | 320 | 2620 | 3170 | 550 | 2840 | 3260 | 420 | Total |  | 850 | Total |  | 885 | %age of 3270 rural link |  | 26.0% | %age of 3270 rural link |  | 27.1% |
| Overtaking Sections                                |   |                     |                         |              |            |  |  |            |  |  |            |  |  |                |              |            |                |              |            |     |     |     |     |     |     |      |      |     |      |      |     |      |      |     |      |      |     |       |  |     |       |  |     |                         |  |       |                         |  |       |
| Northbound   |   |                     | Southbound              |              |            |  |  |            |  |  |            |  |  |                |              |            |                |              |            |     |     |     |     |     |     |      |      |     |      |      |     |      |      |     |      |      |     |       |  |     |       |  |     |                         |  |       |                         |  |       |
| Start Chainage                                     | End Chainage  | Length (m)          | Start Chainage          | End Chainage | Length (m) |  |  |            |  |  |            |  |  |                |              |            |                |              |            |     |     |     |     |     |     |      |      |     |      |      |     |      |      |     |      |      |     |       |  |     |       |  |     |                         |  |       |                         |  |       |
| 355  | 510   | 155                 | 575                     | 430          | 145        |  |  |            |  |  |            |  |  |                |              |            |                |              |            |     |     |     |     |     |     |      |      |     |      |      |     |      |      |     |      |      |     |       |  |     |       |  |     |                         |  |       |                         |  |       |
| 2340   | 2485  | 145                 | 2740                    | 2420         | 320        |  |  |            |  |  |            |  |  |                |              |            |                |              |            |     |     |     |     |     |     |      |      |     |      |      |     |      |      |     |      |      |     |       |  |     |       |  |     |                         |  |       |                         |  |       |
| 2620   | 3170  | 550                 | 2840                    | 3260         | 420        |  |  |            |  |  |            |  |  |                |              |            |                |              |            |     |     |     |     |     |     |      |      |     |      |      |     |      |      |     |      |      |     |       |  |     |       |  |     |                         |  |       |                         |  |       |
| Total  |   | 850                 | Total                   |              | 885        |  |  |            |  |  |            |  |  |                |              |            |                |              |            |     |     |     |     |     |     |      |      |     |      |      |     |      |      |     |      |      |     |       |  |     |       |  |     |                         |  |       |                         |  |       |
| %age of 3270 rural link                            |   | 26.0%               | %age of 3270 rural link |              | 27.1%      |  |  |            |  |  |            |  |  |                |              |            |                |              |            |     |     |     |     |     |     |      |      |     |      |      |     |      |      |     |      |      |     |       |  |     |       |  |     |                         |  |       |                         |  |       |

|                                 |   |
|---------------------------------|---|
| Associated departures           | None  |
| Justification                   |   |
| Justification for the departure | <p>A design option was considered to increase the length of overtaking section between Sutton Courtenay Roundabout and Abingdon Roundabout, however this option was rejected given the associated construction implications of providing a Full Overtaking Sight Distance (FOSD) vertical curve and the identified benefits of the proposed design in comparison. The implications of increasing the overtaking section on this link include a higher river crossing structure, a large northern embankment, structure drainage issues, and the need for flood plain culverts with attendant maintenance issues. The proposed design will still provide two overtaking sections in each direction between the two roundabouts.</p> <p>A FOSD vertical crest curve with K value of 285 would be necessary to increase the overtaking section between the two roundabouts such that the vertical alignment would rise continuously from Sutton Courtenay Roundabout at the south to Abingdon Roundabout at the north. By comparison, the proposed design incorporates a non-overtaking crest located south of the main river structure. As shown in long section drawings 0027 to 0029 in Annex A, the vertical alignment rises from Sutton Courtenay Roundabout across the flood plain, then lowers over the river to the north side, then rises again to Abingdon Roundabout.</p> <p>The design at this location will incorporate an approach viaduct above the flood plain on the south side of the river plus a main crossing structure over the river itself, with an overall structure length of 500m. For details, refer to general arrangement drawings in Annex A.</p> <p>In terms of benefits, the proposed design:</p> <ul style="list-style-type: none"> <li>• raises the height of the approach viaduct across the flood plain which benefits flood conveyancing capacity and bridge pier arrangements</li> <li>• creates more open bridge spans and removes the need for culverts through embankment which would have attendant flood conveyance and maintenance issues</li> <li>• lowers the height of the bridge piers at the main river crossing structure</li> <li>• reduces the embankment height on the north side of the river</li> <li>• introduces a cutting north of the river which creates fill material and reduces imported fill requirements</li> <li>• improves the drainage design on the 500m long structure such that the deck can be drained both to the north and south, reducing the required drainage system capacity and negating the need to shed runoff directly to the flood plain below</li> <li>• brings cost savings associated with benefits noted above</li> <li>• still provides overtaking sections in both directions, to the south between Sutton Courtenay Roundabout and the non-overtaking crest, and between the non-overtaking crest and Abingdon Roundabout to the north.</li> </ul> <p>Given the benefits of the proposed design compared to the implications of altering the road alignment and structures design to increase the overtaking value, plus the associated cost implications, it is recommended that a Departure from Standard is accepted by OCC.</p> |

|  |  |
|--|--|
| Mitigation measures included <i>(if any)</i> | Centreline road markings to be provided to clearly define overtaking sections, with hazard warning lines to indicate where overtaking may not be safe. |
| Mitigation measures rejected <i>(if any)</i> | None   |



## Response and Sign-Off

### Requested departure is agreed for inclusion in the design

If agreement is subject to any condition(s), provide details and rationale:

If not agreed, state ground(s):

Principal Project Sponsor Signature: **Aron Wisdom** Digitally signed by Aron Wisdom  
Date: 2021.01.22 12:11:38 Z Date: \_\_\_\_\_

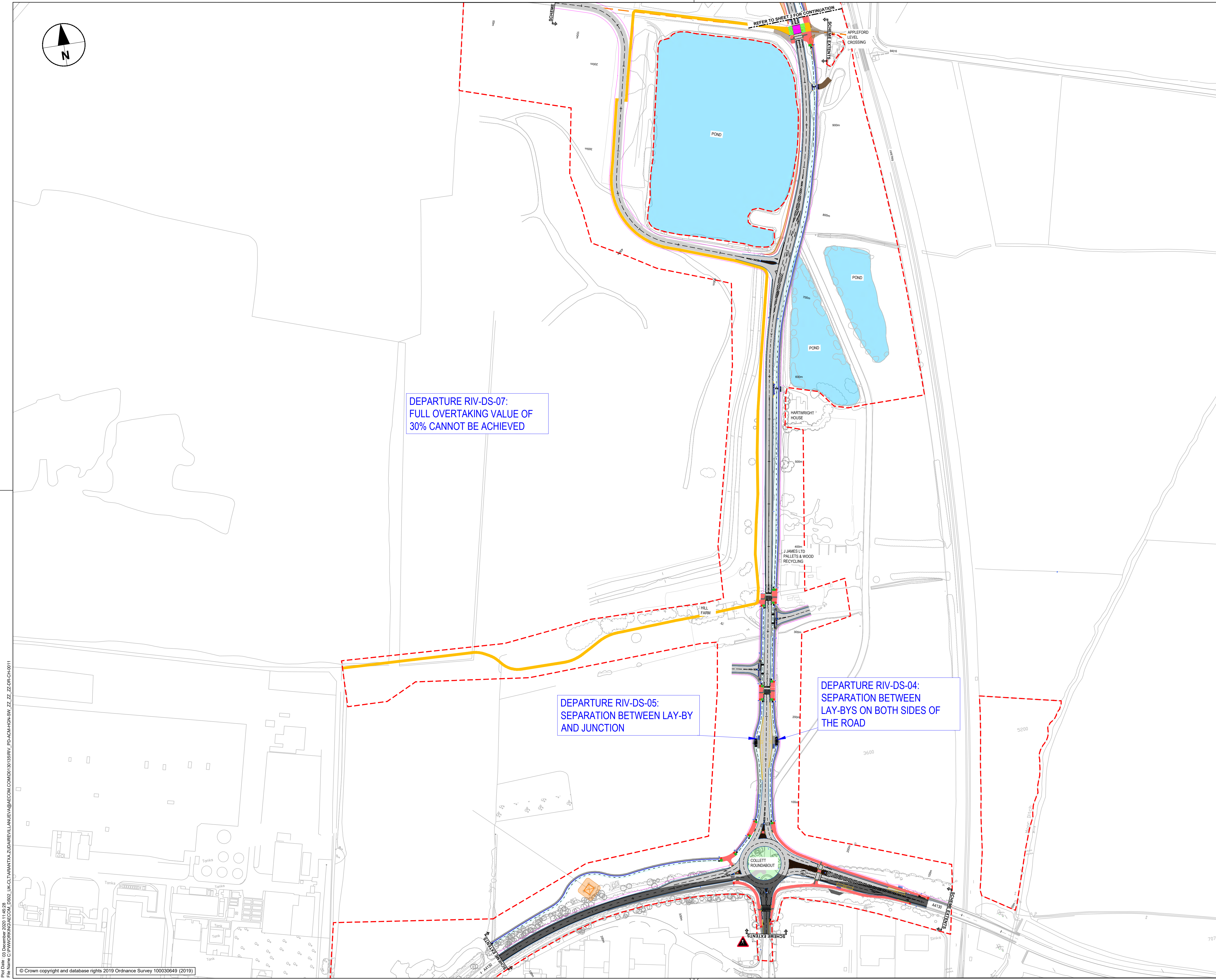
Assistant Director for Highways and Transport Signature:  Date: 22<sup>nd</sup> March 2021

*I can confirm I approve and happy with proposed departure, in particular as any post scheme issue could be mitigated through reduced speed limited or no overtaking if identified as required.*

# Annex A – Schedule of submitted drawings, reports and supporting documentation

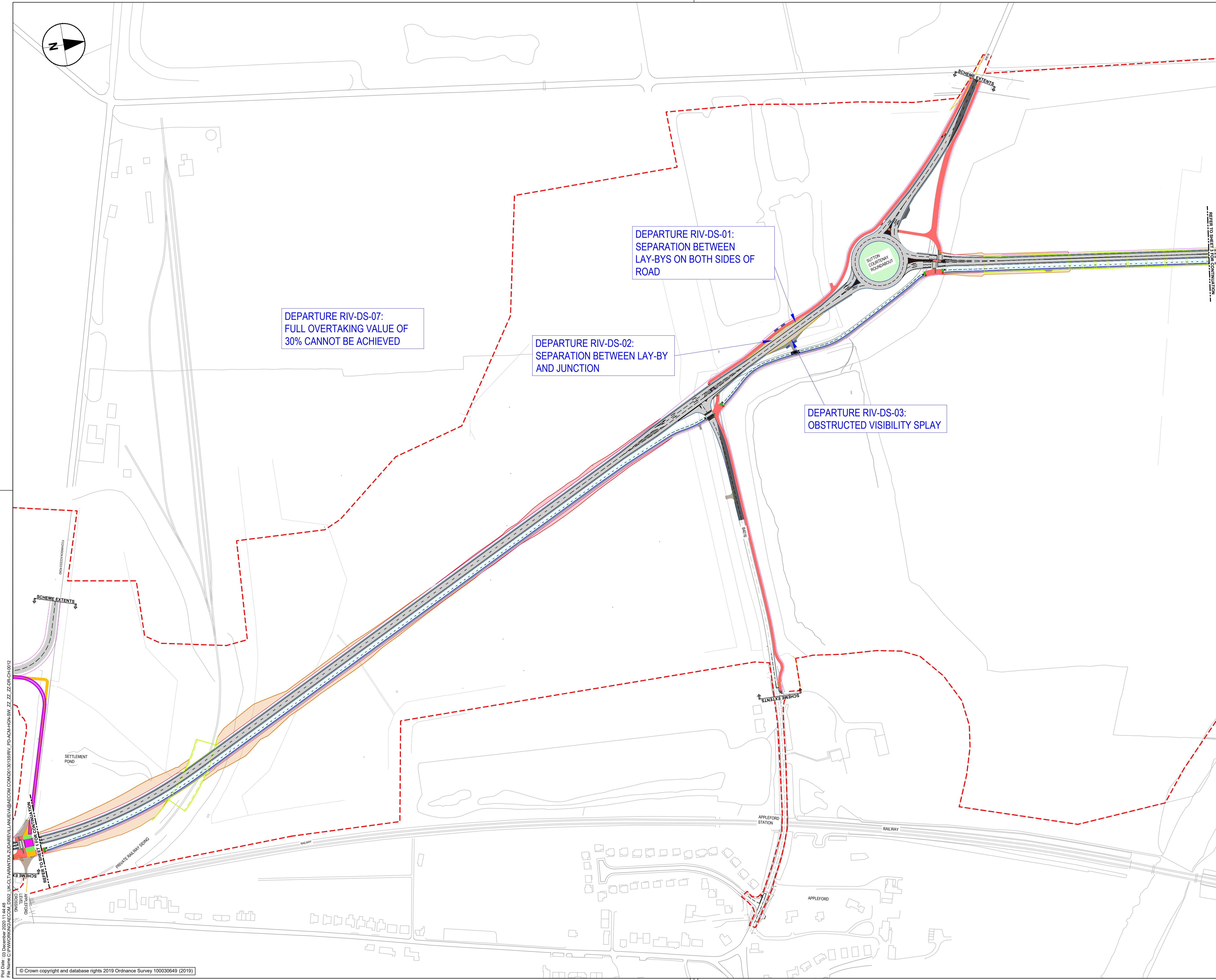
| Title  | Attachments                    | Version                 |
|--|--------------------------------|-------------------------|
| <i>Drawings (relating to the departure)</i>  |                                |                         |
| Preliminary Design, Departures, Sheets 1 to 3 of 3<br>RIV_PD-ACM-HGN-SW_ZZ_ZZ_ZZ-DR-CH-0011<br>RIV_PD-ACM-HGN-SW_ZZ_ZZ_ZZ-DR-CH-0012<br>RIV_PD-ACM-HGN-SW_ZZ_ZZ_ZZ-DR-CH-0014                                    | Attached                       | P02                     |
| Preliminary Design, Long sections, Sheets 5 to 7 of 13<br>RIV_PD-ACM-HGA-SW_ZZ_ZZ_ZZ-DR-CH-0027<br>RIV_PD-ACM-HGA-SW_ZZ_ZZ_ZZ-DR-CH-0028<br>RIV_PD-ACM-HGA-SW_ZZ_ZZ_ZZ-DR-CH-0029                                | Attached                       | P02                     |
| River Thames Crossing, Option Study,<br>General Arrangement, Sheets 1 to 3 of 3<br>RIV_PD-ACM-SBR-DGT_STR_ZZ_ZZ-DR-CB-0021<br>RIV_PD-ACM-SBR-DGT_STR_ZZ_ZZ-DR-CB-0022<br>RIV_PD-ACM-SBR-DGT_STR_ZZ_ZZ-DR-CB-0023 | Attached                       | P01.2<br>P01.1<br>P01.1 |
| <i>Reports (relating to the departure)</i>   |                                |                         |
|  |                                |                         |
|  |                                |                         |
|  |                                |                         |
| <i>Supporting documentation (for information)</i>  |                                |                         |
| DMRB CD 109 Revision 1 - March 2020  | <a href="#">Link to CD 109</a> | Rev 1                   |
|  |                                |                         |
|  |                                |                         |





|                                   |  |  |              |  |  |          |  |  |          |  |  |
|-----------------------------------|--|--|--------------|--|--|----------|--|--|----------|--|--|
| Drawing Number<br>Work Package ID |  |  | I Originator |  |  | I Volume |  |  | Rev      |  |  |
| RIV_PD                            |  |  | -ACM         |  |  | -HGN -   |  |  | P02      |  |  |
| SW_ZZ_ZZ_ZZ                       |  |  | -DR          |  |  | -CH-0011 |  |  |          |  |  |
| Location                          |  |  | I Type       |  |  | I Role   |  |  | I Number |  |  |





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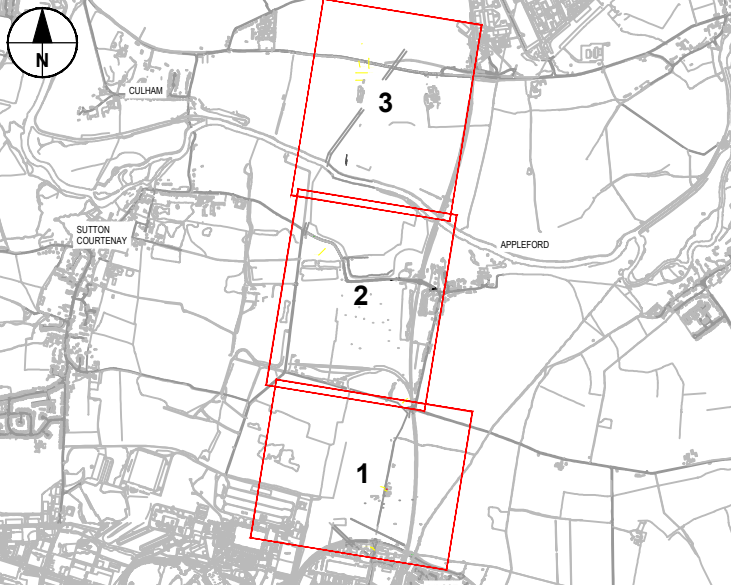
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PRELIMINARY DESIGN DRAWING  
NO CONSTRUCTION RISKS TO NOTE IN CURRENT REVISION

FOR MAINTENANCE / OPERATION / DECOMMISSIONING AND DEMOLITION RISKS SEE PROJECT HEALTH AND SAFETY FILE.

- KEY**
- REDLINE BOUNDARY
  - PROPOSED CARRIAGEWAY FULL DEPTH CONSTRUCTION
  - PROPOSED CARRIAGEWAY TRANSITION CONSTRUCTION FROM EXISTING TO PROPOSED CARRIAGEWAY
  - PROPOSED ASPHALT FOOTWAY
  - PROPOSED CYCLEWAY
  - PROPOSED SHARED USE FOOTWAY / CYCLEWAY
  - PROPOSED GRASS VERGE/LANDSCAPING
  - PROPOSED ROAD MARKINGS

**DRAFT**



|                  |  |    |       |          |        |
|------------------|--|----|-------|----------|--------|
| FIRST ISSUE      |  | AZ | KC    | 27/10/20 | P01    |
| DEP 07 ADDED     |  | AZ | KC    | 03/12/20 | P02    |
| REVISION DETAILS |  | By | Check | Date     | Suffix |

Purpose of issue  
**SUITABLE FOR INFORMATION**



Project Title  
**RIVER CROSSING  
DIDCOT GARDEN TOWN  
HIF 1 SCHEMES**

Drawing Title  
**HIGHWAYS  
PRELIMINARY DESIGN  
DEPARTURES  
SHEET 2 OF 3**

|                                  |             |                                |                |                  |
|----------------------------------|-------------|--------------------------------|----------------|------------------|
| Designed<br>AZ                   | Drawn<br>AZ | Checked<br>HP                  | Approved<br>KC | Date<br>03/12/20 |
| Internal Project No.<br>60632497 |             | Suitability<br>S2              |                |                  |
| Scale @ A1<br>1:2000             |             | Discipline<br>Civil - Highways |                |                  |

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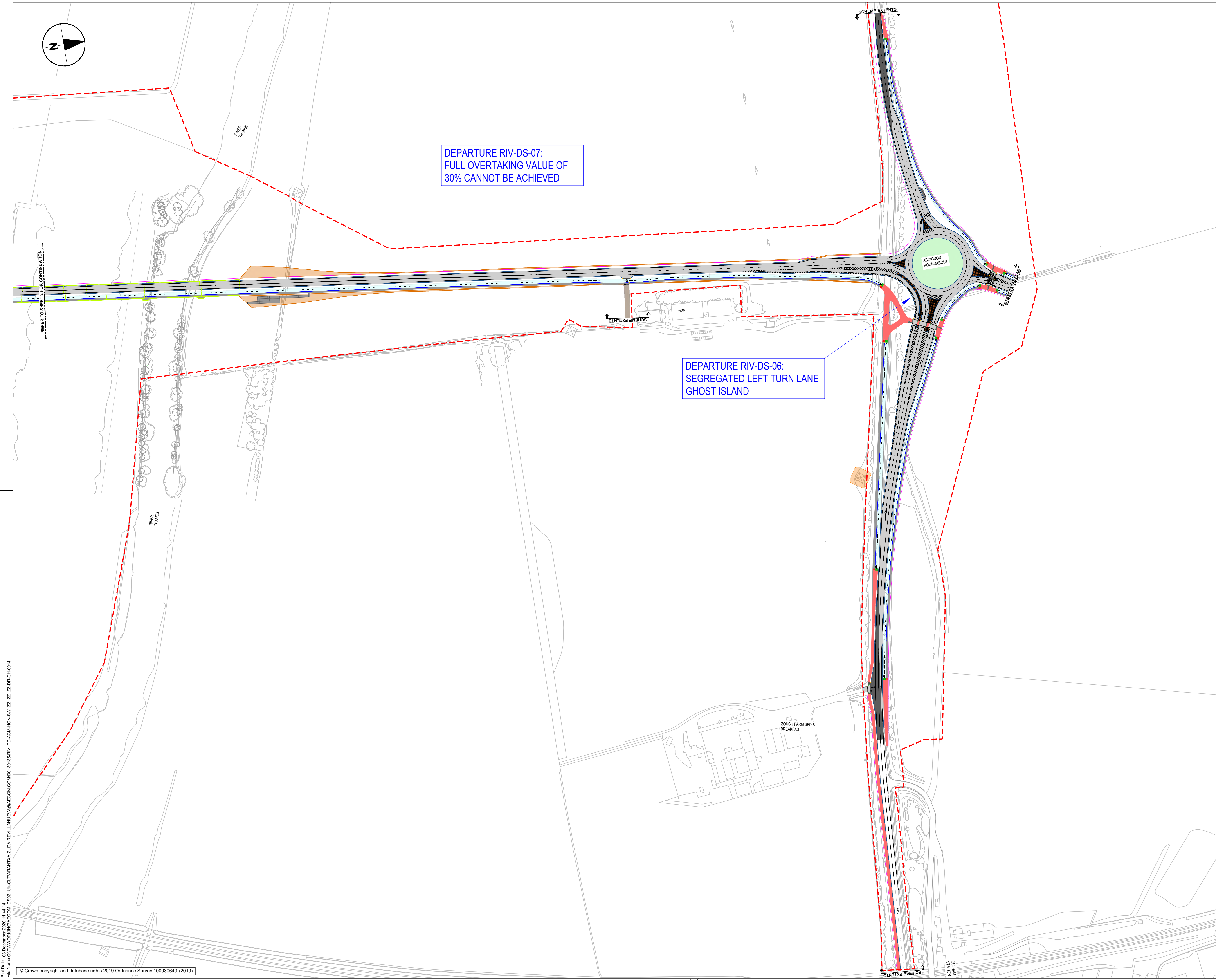
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63-77 Victoria Street  
St Albans  
Herts  
AL1 3ER  
Tel: +44 (0) 172 7535 000  
www.aecom.com

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|---------------------------------|--------------------------------|-----------------------------|--------------------------------|------------------------|
| Drawing Number<br><b>RIV_PD</b> | Work Package ID<br><b>-ACM</b> | 1 Originator<br><b>-HGN</b> | 1 Volume<br><b>-DR-CH-0012</b> | Rev<br><b>P02</b>      |
| Location<br><b>SW_ZZ_ZZ_ZZ</b>  |                                |                             |                                | 1 Type 1 Role 1 Number |









Rev Date 03 December 2020 11:24:45  
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DEPARTURE RIV-DS-06:  
SEGREGATED LEFT TURN LANE  
GHOST ISLAND

**KEY**

-  REDLINE BOUNDARY
-  PROPOSED CARRIAGEWAY FULL DEPTH CONSTRUCTION
-  PROPOSED CARRIAGEWAY TRANSITION CONSTRUCTION FROM EXISTING TO PROPOSED CARRIAGEWAY
-  PROPOSED ASPHALT FOOTWAY
-  PROPOSED CYCLEWAY
-  PROPOSED SHARED USE FOOTWAY / CYCLEWAY
-  PROPOSED GRASS VERGE/LANDSCAPING
-  PROPOSED ROAD MARKINGS

|                  |                          |
|------------------|--------------------------|
| Purpose of issue | SUITABLE FOR INFORMATION |
|------------------|--------------------------|



|                                  |             |                                |                |                  |
|----------------------------------|-------------|--------------------------------|----------------|------------------|
| Designed<br>AZ                   | Drawn<br>AZ | Checked<br>HP                  | Approved<br>KC | Date<br>03/12/20 |
| Internal Project No.<br>60632497 |             | Suitability<br>S2              |                |                  |
| Scale @ A1<br>1:2000             |             | Discipline<br>Civil - Highways |                |                  |

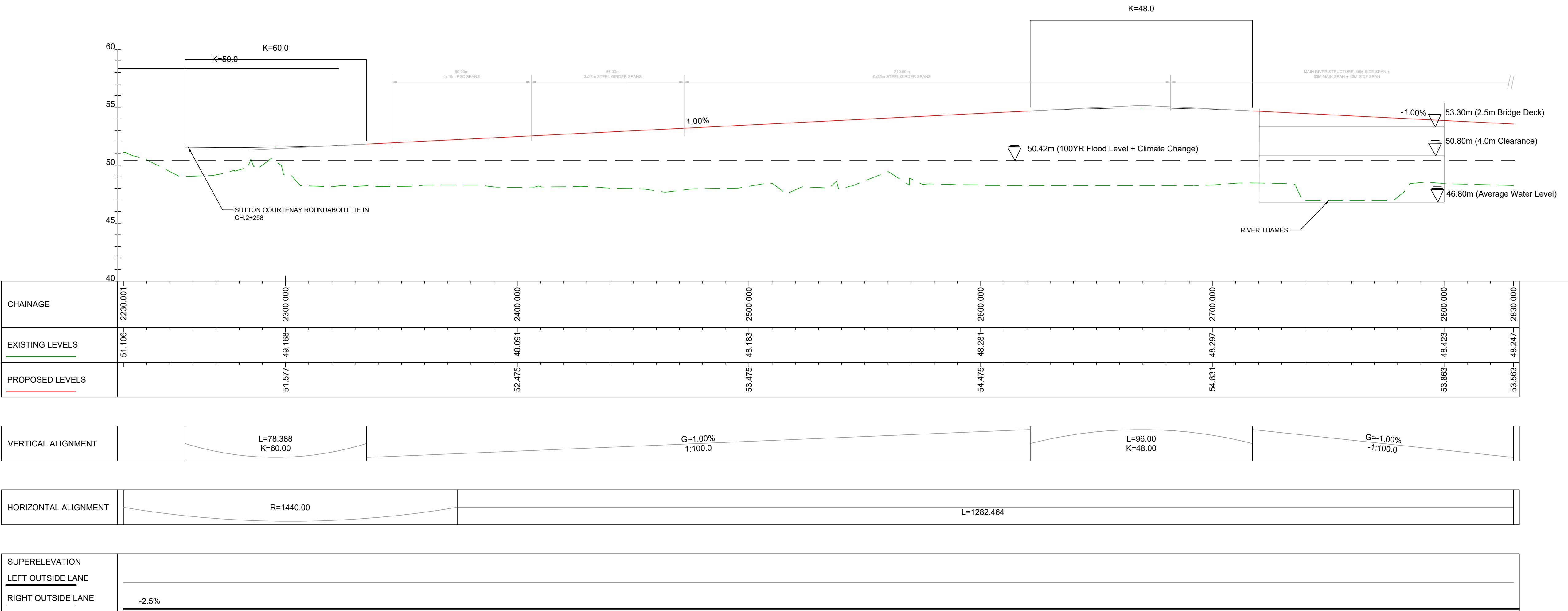
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St Albans  
Herts  
AL1 3ER  
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|--|---|---|-------------------|
| Drawing Number<br>Work Package ID<br><b>RIV_PD</b><br><b>SW_ZZ_ZZ_ZZ</b><br>Location | I Originator<br><b>-ACM</b><br><b>-DR</b><br>I Type | I Volume<br><b>-HGN -</b><br><b>-CH-0014</b><br>I Role I Number | Rev<br><b>P02</b> |
|--|---|---|-------------------|



Rev'd Date: 18 December 2020 12:31:13  
File Name: C:\TOWN\Oxfordshire\CH002\_UK-CL\TIERESA\_TERR09BALZALDE@AECOM.COM\01313SRIV\_PD-ACM-HGA-SW\_ZZ\_ZZ-DR-CH-002



DIDCOT TO CULHAM RIVER CROSSING MAINLINE-2 LONGSECTION  
SCALE: H 1:1000 ; V 1:200  
CHAINAGE: 2230.001m - 2830.000m

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|                   |             |          |        |
|-------------------|-------------|----------|--------|
|                   |             |          |        |
| First Issue       | AZ<br>KC    | 23/10/20 | P01    |
| REVISED ALIGNMENT | AZ<br>KC    | 17/12/20 | P02    |
| REVISION DETAILS  | By<br>Check | Date     | Suffix |

SUITABLE FOR INFORMATION

Client  
County Hall  
New Road  
Oxford  
OX1 1ND



Project Title  
**RIVER CROSSING  
DIDCOT GARDEN TOWN  
HIF 1 SCHEMES**

Drawing Title  
**HIGHWAYS  
PRELIMINARY DESIGN  
LONG - SECTIONS  
SHEET 5 OF 13**

|                                  |                   |                                |                |                  |
|----------------------------------|-------------------|--------------------------------|----------------|------------------|
| Designed<br>AZ                   | Drawn<br>AZ       | Checked<br>DR                  | Approved<br>KC | Date<br>17/12/20 |
| Internal Project No.<br>60632497 | Suitability<br>S2 | Discipline<br>Civil - Highways |                |                  |

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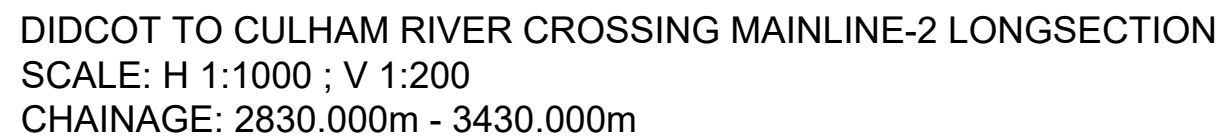
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Croydon  
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| Drawing Number<br>RIV_PD<br>SW_ZZ_ZZ_ZZ | Work Package ID<br>-ACM<br>-HGA -<br>-DR-CH-0027 | 1 Originator<br>1 Volume | Rev<br>P02 |
|---|--|--------------------------|------------|

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SUITABLE FOR INFORMATION

**OXFORDSHIRE  
COUNTY COUNCIL**

# RIVER CROSSING DIDCOT GARDEN TOWN HIF 1 SCHEMES

HIGHWAY  
 PRELIMINARY DESIGN  
 LONG - SECTIONS  
 SHEET 6 OF 13

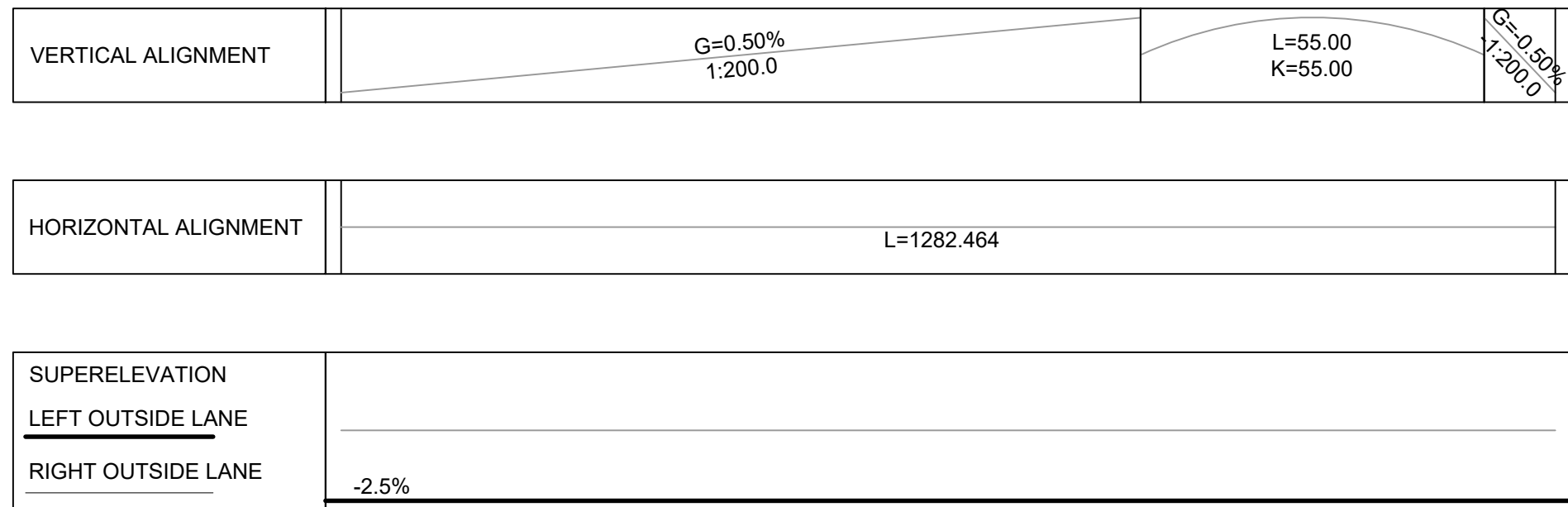
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| Work Package ID |             |        |          |
| I Originator    | I Volume    |        |          |
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| SW_ZZ_ZZ_ZZ     | -DR-CH-0028 |        |          |
| Location        | I Type      | I Role | I Number |

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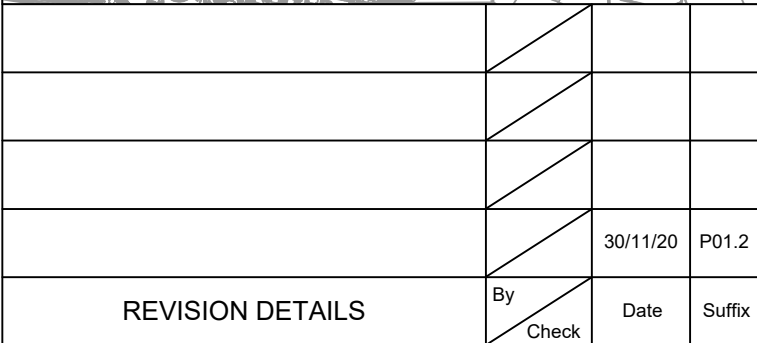
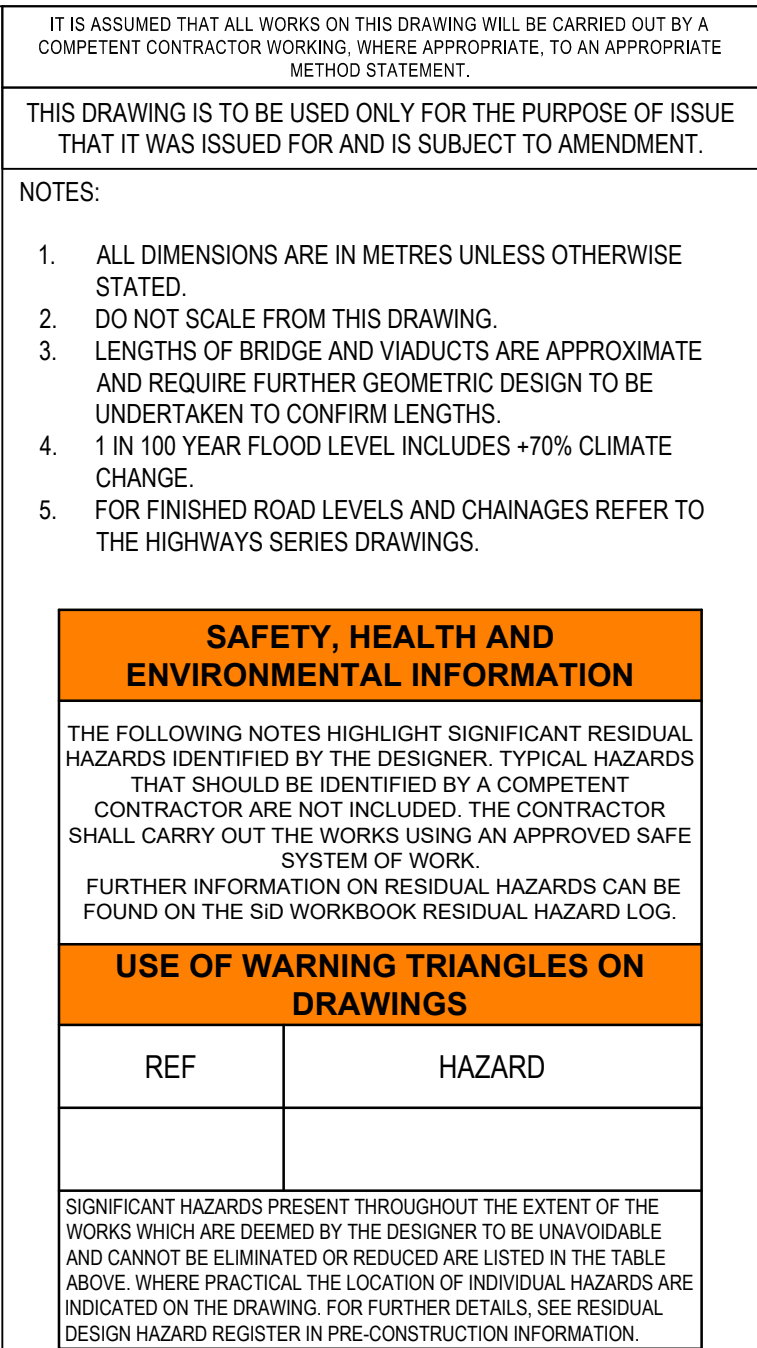
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 Croydon  
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| Drawing Number  |              |          | Rev      |
| Work Package ID | I Originator | I Volume |          |
| RIV_PD          | -ACM         | -HGA -   | P02      |
| SW_ZZ_ZZ_ZZ     | -DR-CH-0029  |          |          |
| Location        | I Type       | I Role   | I Number |





INITIAL STATUS OR WIP

Client

 **OXFORDSHIRE  
COUNTY COUNCIL**

Project Title

**DIDCOT GARDEN TOWN  
HIF 1 SCHEMES  
PRELIMINARY DESIGN**

Drawing Title

RIVER THAMES CROSSING  
OPTION STUDY  
GENERAL ARRANGEMENT  
SHEET 1

|                                  |              |   |                 |             |
|----------------------------------|--------------|---|-----------------|-------------|
| Designed<br>GT                   | Drawn<br>--- | Checked<br>---                              | Approved<br>--- | Date<br>--- |
| Internal Project No.<br>60632497 |              | Suitability<br>S0                           |                 |             |
| Scale @ A1<br>AS SHOWN           |              | Discipline<br>Civil - Bridge and Structures |                 |             |

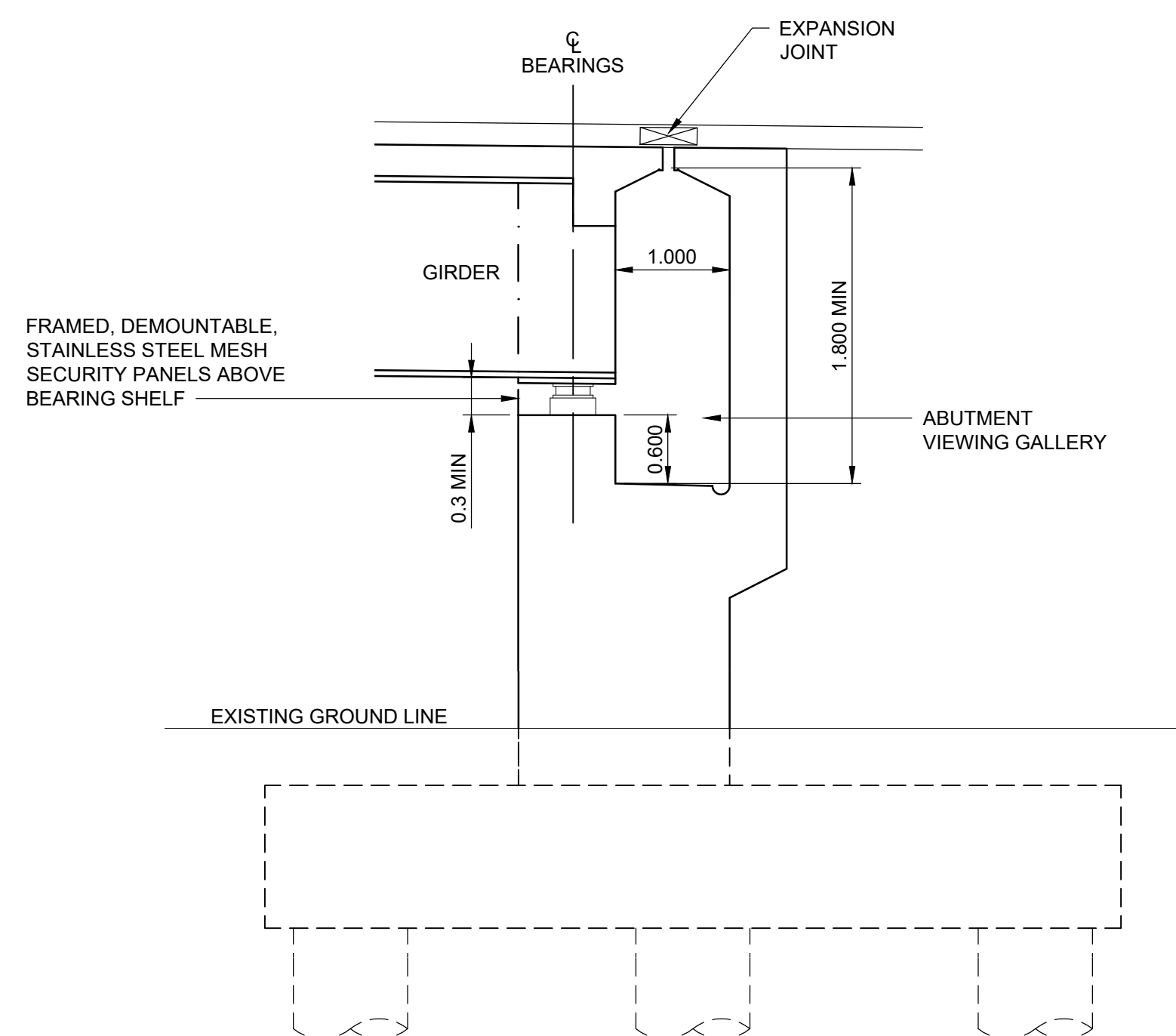
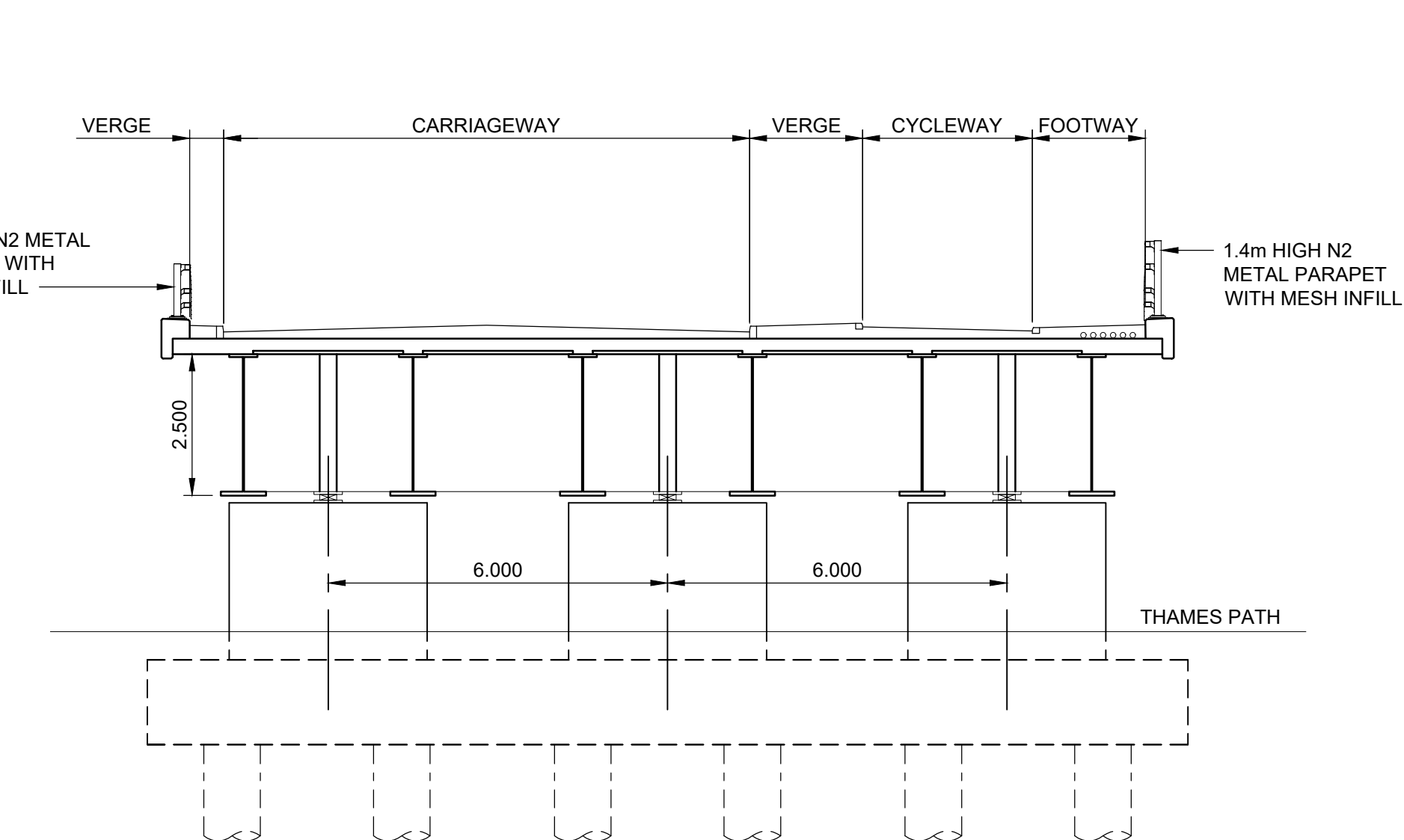
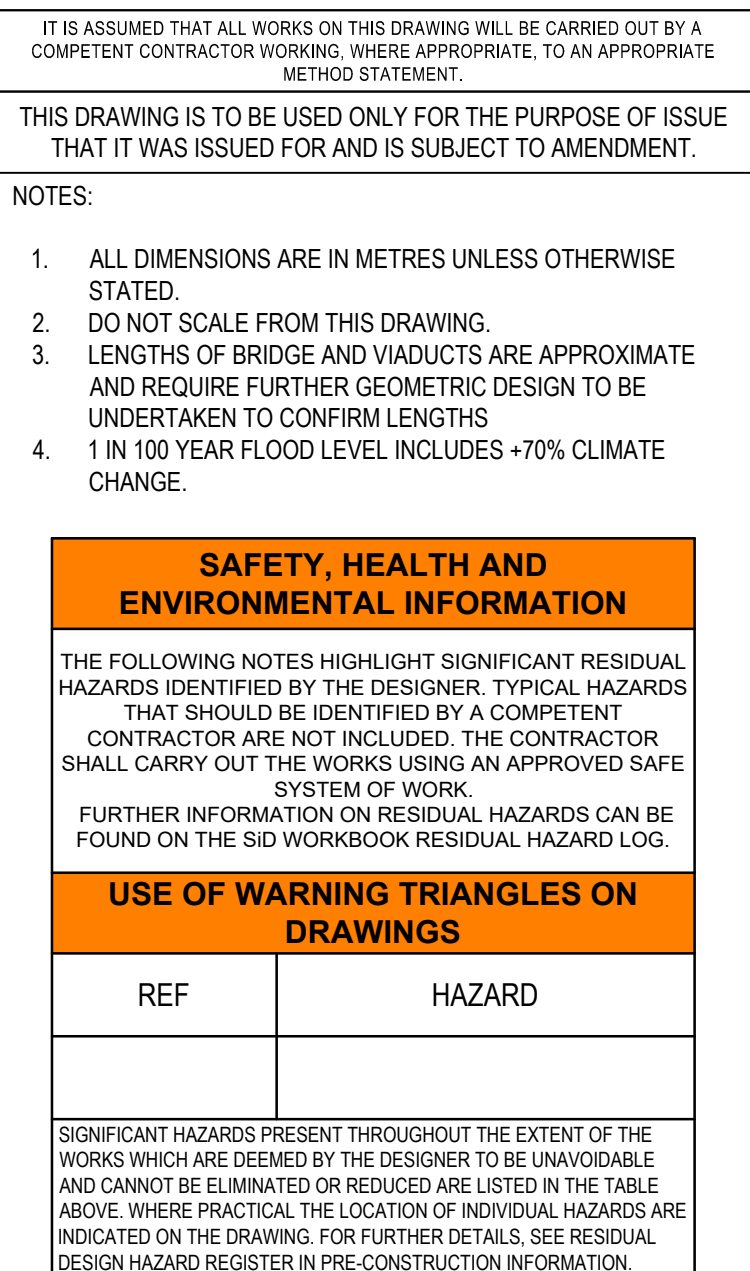
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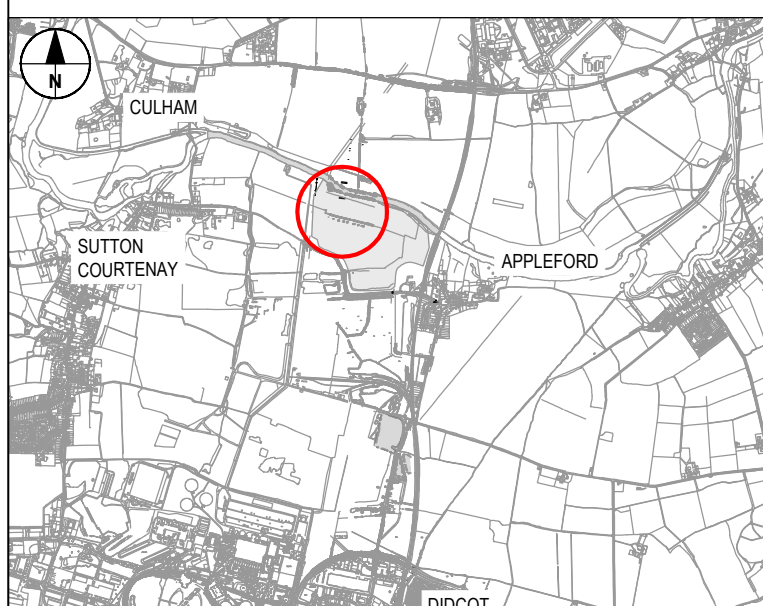
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| Work Package ID |  |              |  |          |  |          |
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| DGT_STR_ZZ_ZZ   |  | -DR          |  | -CB-0021 |  |          |
| Location        |  | I Type       |  | I Role   |  | I Number |





| CHECK PRINT           |      |         |
|-----------------------|------|---------|
| CHECK                 | DATE | INITIAL |
| DRAFTING              |      |         |
| ENGINEERING           |      |         |
| CORRECTIONS COMPLETED |      |         |
|                       |      |         |



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|------------------|-------------|----------|--------|
|                  |             |          |        |
|                  |             |          |        |
|                  |             |          |        |
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|                  |             | 24/11/20 | P01.1  |
| REVISION DETAILS | By<br>Check | Date     | Suffix |

| Purpose of issue | INITIAL STATUS OR WIP |
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| Project Title |  |
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DIDCOT GARDEN TOWN  
HIF 1 SCHEMES  
PRELIMINARY DESIGN

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Drawing Title

RIVER THAMES CROSSING  
OPTION STUDY  
GENERAL ARRANGEMENT  
SHEET 2

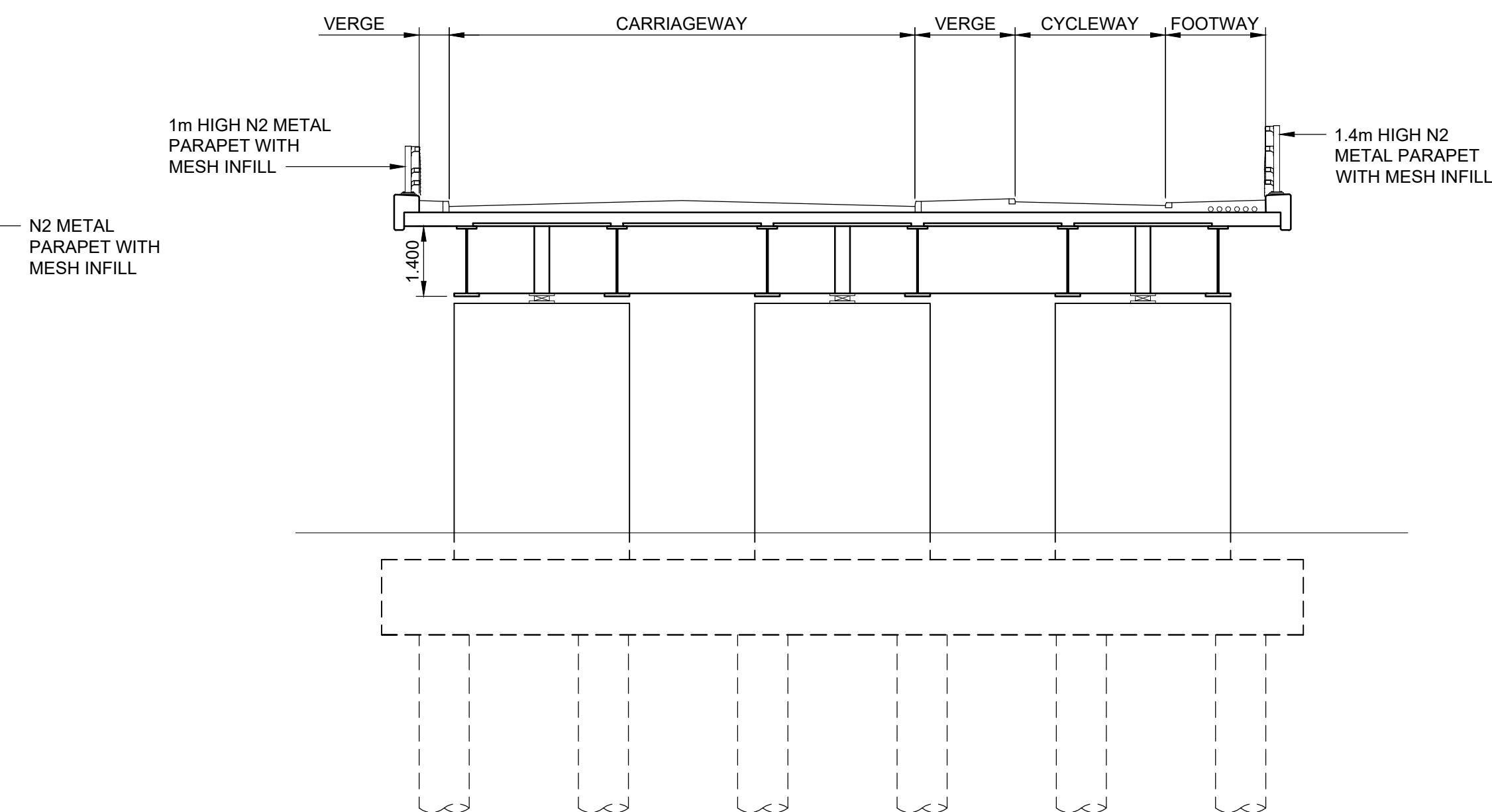
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| Scale @ A1<br>AS SHOWN           |              | Discipline<br>Civil - Bridge and Structures |                 |             |

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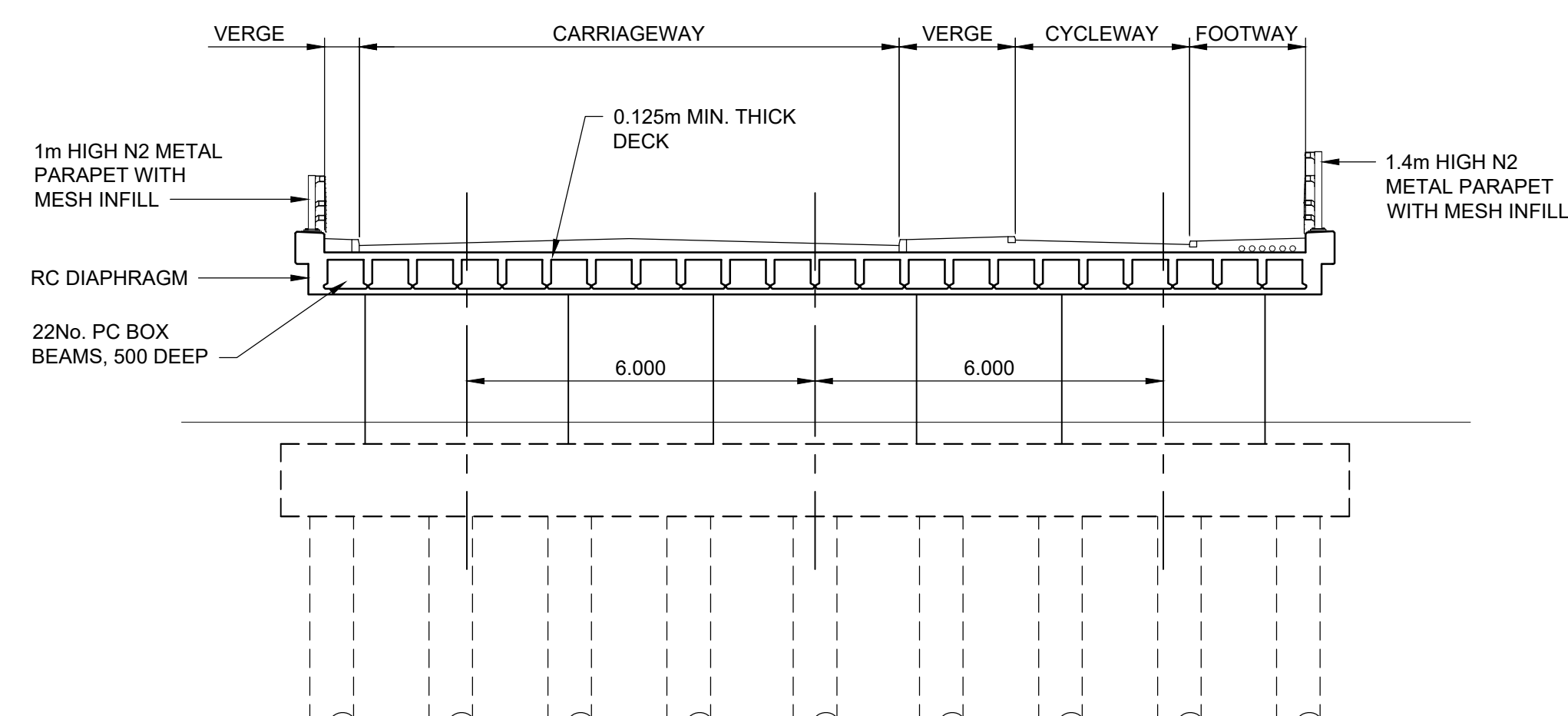
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| Drawing Number  |  |              | Rev      |          |       |
| Work Package ID |  | I Originator | I Volume |          |       |
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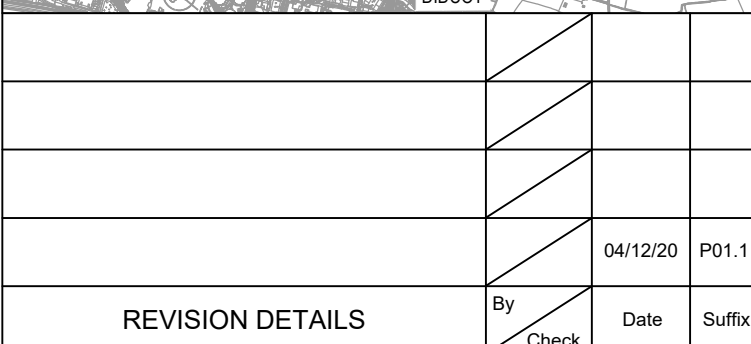
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| <b>F</b> | <b>SECTION</b> |
| 0021     | Scale 1:100    |



|          |                |
|----------|----------------|
| <b>H</b> | <b>SECTION</b> |
| 0021     | Scale 1:100    |

| SAFETY, HEALTH AND ENVIRONMENTAL INFORMATION  |   |
|---|---|
| <p>THE FOLLOWING NOTES HIGHLIGHT SIGNIFICANT RESIDUAL HAZARDS IDENTIFIED BY THE DESIGNER. TYPICAL HAZARDS THAT SHOULD BE COVERED BY A COMPETENT CONTRACTOR ARE NOT INCLUDED. THE CONTRACTOR SHALL CARRY OUT THE WORKS USING AN APPROVED SAFE SYSTEM OF WORK.</p> <p>FURTHER INFORMATION ON RESIDUAL HAZARDS CAN BE FOUND ON THE SID WORKBOOK RESIDUAL HAZARD LOG.</p> |   |
| USE OF WARNING TRIANGLES ON DRAWINGS  |   |
| REF   | HAZARD  |
|   | <p>SIGNIFICANT HAZARDS PRESENT THROUGHOUT THE EXTENT OF THE WORKS WHICH ARE DESIGNER BY THE DESIGNER TO BE UNAVOIDABLE AND CANNOT BE ELIMINATED OR REDUCED ARE LISTED IN THE TABLE ABOVE. WHERE PRACTICAL, THE LOCATION OF INDIVIDUAL HAZARDS ARE INDICATED ON THE DRAWING. FOR FURTHER DETAILS, SEE RESIDUAL DESIGN HAZARD REGISTER IN PRE-CONSTRUCTION INFORMATION.</p> |



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| Project Title |  |
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Drawing Title

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