

City Airport Development Programme (CADP1)

Condition 96: Construction Compound



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1 Introduction

- 1.1 The City Airport Development Programme (CADP1) planning application (13/01228/FUL) was granted planning permission by the Secretaries of State for Communities and Local Government and Transport in July 2016 following an appeal and public inquiry which was held in March/April 2016. Condition 96 of the CADP1 permission is as follows:

“Before the commencement of development details of the construction compounds and any associated hoarding shall be submitted for approval in writing by the Local Planning Authority. Development shall be carried out in accordance with the approved details.

Reason: To ensure a satisfactory standard of development and to safeguard the amenities of the surrounding area.”

- 1.2 Details pursuant to Condition 96 were previously approved in April 2017 (ref. 17/00530/AOD). Since then, the Airport’s Delivery Partner have identified some small changes to the overall compound area to accommodate the necessary facilities and lay down areas for the appointed CADP1 contractors.
- 1.3 This submission seeks approval of revised details pursuant to Condition 96 as described below and shown in Appendix 1; Figures 1 & 2.
- 1.4 At the request of LBN Officers, new text added to the previously approved details (17/00530/AOD) has been distinguished in blue text in this document.
- 1.5 The Updated Environmental Statement (UES) accompanying the CADP1 planning application included an indicative location of the main dockside Construction Compound at the eastern end of the Airport. UES Chapter 6: Development Programme and Construction, described that this compound would include: offices; welfare facilities; parking; materials laydown; and on-site fabrication. It was assumed that precast elements for the deck structure would be manufactured off-site at a location to be determined once a contractor appointed.
- 1.6 The location of the proposed main Construction Compound, relative to that indicatively shown in the UES, remains the same. Whilst the size of the compound will vary over time as required throughout the CADP1 build, the maximum extent required has increased slightly from approximately 1.62Ha (approval ref. 17/00530/AOD) to approximately 2Ha. . The area of compound in active use at any one time is likely to be substantially smaller than the total compound area and will move from one area to another depending on the location and type of construction activities occurring. The larger total area reflects the boundary within which this will occur. The total area would not be fully occupied at any point.
- 1.7 The slight increase in the maximum area is required for the following reasons:
- to give flexibility for multiple contractors to occupy different areas of the compound relative to the works they are carrying out;
 - to provide additional area for lay-down and storage required by onsite contractors;

- to allow sufficient space for bringing materials to and from site using barges (aligning with Condition 60 – Use of the River Thames); and
- to allow flexibility in the compound area to provide for temporary uses during construction such as a temporary taxi feeder park and redistribution of car parking provision.

1.8 In addition, a separate Contractor's Compound will be established in the West Service Yard to facilitate the construction of the West Terminal Extension (WTE) (see Appendix 1; Figure 1). This smaller compound will secure and shield the WTE construction works from the public, passengers and other users of the main terminal building and provide the appointed Contractor with a dedicated facility. However, it is expected that due to limitation of space, bulk construction materials for the WTE works will be delivered to, and stored in, the main Construction Compound until required.

2 Construction Compound – Dockside

- 2.1 The proposed Construction Compound extends to c. 2 Ha and will be located east of the Airport at KGV Dock immediately north of Hartmann Road (see Appendix 1; Figure 2). It will be accessible to construction vehicles via Hartmann Road from Woolwich Manor Way, and by barges entering the dock via the dock gate, as previously shown in Figure 6.11 of the UES and reproduced in Appendix 2.
- 2.3 This document seeks approval for the full extent of the compound area shown on Figure 2. The compound area will be used as the primary logistics hub for the CADP1 works and will include:
- Site cabins (security/contractors' offices & meeting rooms);
 - Welfare facilities (toilets, washdown & drying rooms);
 - Construction materials and component storage;
 - Contractor vehicle parking;
 - Construction plant and machinery (whilst not in use);
 - On-site construction manufacturing (such as welding, joinery); and
 - A maintenance workshop.
- 2.4 The compound will also have two barge berthing points which will be the locations for loading and off-loading barges used for bringing materials to and from the compound. The berthing points will also be used for loading (and un-loading) plant and materials onto barges used for the airfield piling and deck works.
- 2.5 A substantial area of the compound will also be utilised for materials and waste storage. The size and location of this area will vary throughout the construction period. As required under Condition 60 of the CADP1 permission, LCA is seeking to maximise the transport of waste and construction materials via the River Thames. Consequently, large barge loads are expected and additional space is required for laydown and storage of related materials (incoming) and waste (outgoing).
- 2.6 In summary, the storage area will comprise the following:
- Loading/off-loading platforms over the adjacent 'Dolphins' in KGV Dock;
 - Construction aggregate storage area;
 - Waste storage area (segregated by type);
 - Storage of construction materials and components (steel, piles, pre-cast concrete planks etc);
 - Concrete cast beam storage area; and
 - General construction plant laydown area.
- 2.7 The grey hatched areas shown on the compound drawings (Appendix 1; Figure 2) identify the location of the barge berthing points for both off & on-loading materials across Dolphins 1 and 2.

- 2.8 Materials that will typically be brought in by barge (and therefore requiring storage/laydown) will be:
- Construction aggregates;
 - Steel piles;
 - Steel for rebar;
 - Structural steel;
 - Cladding; and
 - Timber for formwork / shuttering.
- 2.9 Materials brought in by barge will be off-loaded at berthing locations using mobile cranes and transported using telehandlers, or similar plant, to the laydown area. Barges will be brought into the dock and berthed during normal working hours.
- 2.10 Construction plant/equipment and materials for the construction of the piling and deck works will be loaded onto barges at the berthing points prior to being transported to the area of works. Loading will be planned during the daytime (prior to the works shift) to ensure works Outside of Operation Hours (OOOH) are minimised insofar as is practicable.
- 2.11 Although the piling and deck works shall endeavour to load during the day there may be times when barges have to be reloaded during the OOOH, for example when incorrect materials are loaded and/or additional materials are required; and where plant breaks down. Such instances are expected to be infrequent. The infrequent use of barges during OOOH does not change the conclusions of the noise assessment in the UES or that carried out under Condition 4 (Construction Phasing Plan).
- 2.12 The [storage area](#) will also enable the appointed contractors to segregate waste materials in bulk prior to being loaded onto barges and transported off site to appropriate waste treatment and recycling facilities. This 'at source' waste segregation not only maximises the use of the River Thames (in accordance with CADP1 Condition 60) but also reduces the environmental impact by lessening the need for the recipients of the waste to re-categorise the material and transport it elsewhere.
- 2.13 Barges are likely to be used to remove scrap materials such as inert demolition waste, broken-out concrete and tarmac, and spoil from the piling and excavations for foundations. The precise quantity of such waste is yet to be determined. However, all inert waste will be segregated and stockpiled for as short a period of time as possible before removal for re-use, recycling or disposal off-site.
- 2.14 The process will also facilitate appropriate on-site segregation and testing for contaminants, as and when required. If the waste is found to be contaminated with any hazardous compounds it will be disposed of at a licensed landfill in the Thames Estuary or elsewhere.

Construction Noise Barrier

- 2.15 The Contractors Compound will be fully enclosed to the south by a 3 metre high construction noise barrier, already build out in accordance with the details approved under condition 94 of the CADP1 permission. The north of the compound will be secured by a 3 metre high mesh fence. A further construction noise barrier will be erected south of Hartmann Road to protect

the residential properties at Woodman Street from construction noise related to the Contractors Compound. This noise barrier was included as a mitigation measure in Chapter 8 (Noise) of the UES.

Construction Compound Hoarding

- 2.16 A temporary construction hoarding will be installed around the Contractors Compound as shown at Appendix 1; Figure 2. This alignment accords with the construction noise barrier already in place. The construction compound hoarding will be no less than 3m in height above local ground level.

3 Contractor Compound - West Service Yard

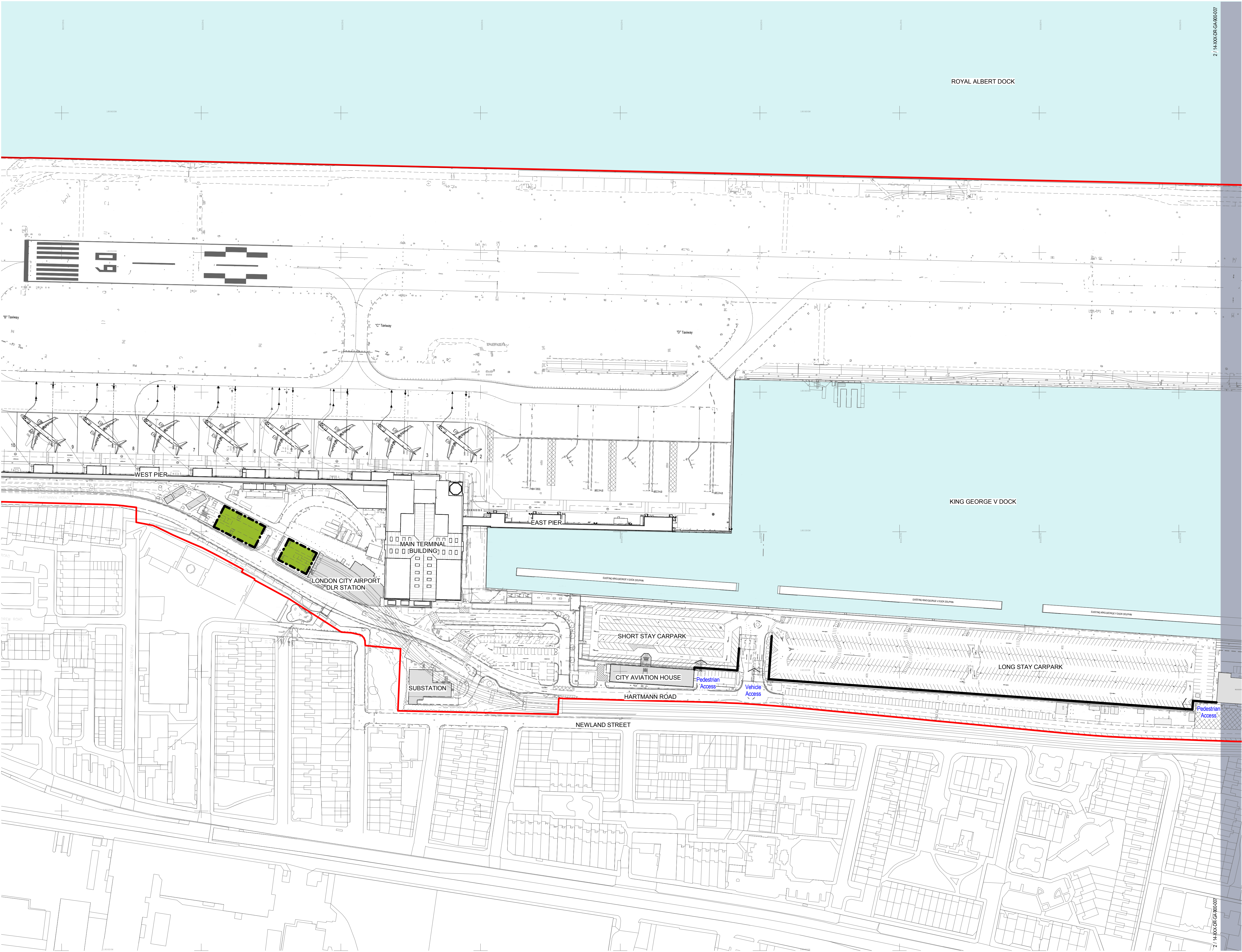
- 3.1 Temporary construction hoarding will be installed in the Western Service Yard area which will provide site security and noise abatement for the local residents to the south of the site. Within the demise of the site there will be a small welfare area for the Contractor's staff/labour; with the principal contractor compound located at the Eastern end of the LCA campus. The site will also be used for plant, materials and equipment storage whilst the WTE and WEC are being constructed.
- 3.2 Access to the DLR assets (incl. back of house) shall be maintained throughout the CADP1 development.

4 Appendices

Appendix 1 - Construction Compound & Details (for approval):

[Figure 1_A400-PAW-A-14-XXX-DR-GA-900-006-C-S3](#)

[Figure 2_A400-PAW-A-14-XXX-DR-GA-900-007-D-S3](#)



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- Base building survey information by LCY and MSA.

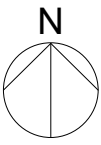
Legend

Indicative Compound Area
Total Area: 0.09278 hectares

Proposed Construction Compound Timber Hoarding and Noise Barrier - 3 metres

Application Boundary

SCALE BAR



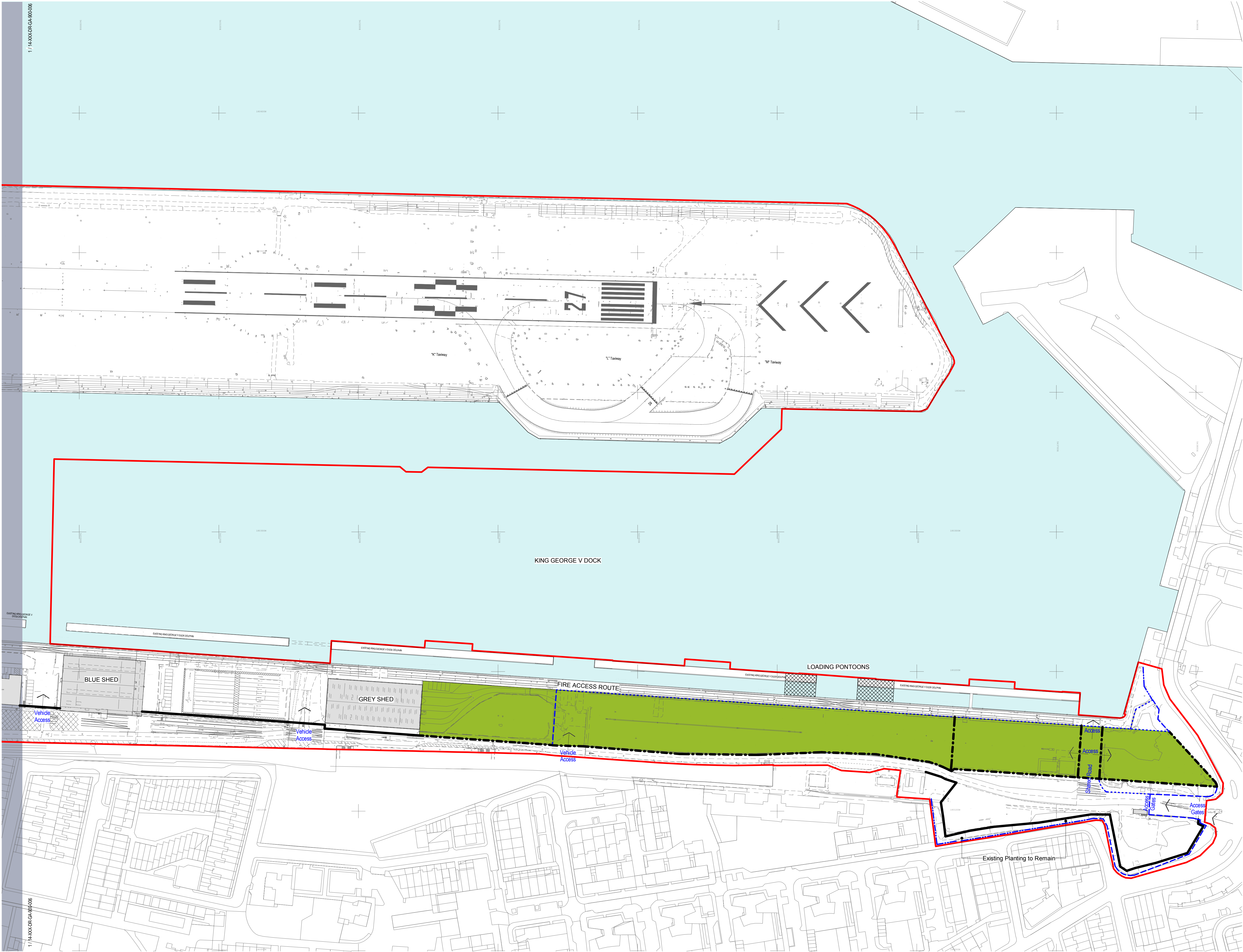
C	MDS	09/01/18	For Approval
Previously issued as A400-PAW-A-13-XXX-DR-GA-948-001			
B	MDS	02/12/16	For Approval
A	MDS	25/10/16	For Planning Approval
Rev	Drn	Date	Description

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Client LONDON CITY AIRPORT						
Project Name CITY AIRPORT DEVELOPMENT PROGRAMME						
Title CADP Condition 96 Figure 1: Construction Compound & Details						
Discipline Architecture				Purpose of Issue For Approval		
Drawing Originator Pascall+Watson architects				Originators Job No. 5077		
Checked By TA		Checked date 09/01/18		Drawn By MS		Drawn Date 25/10/16
Approved By MN		Approval Date 09/01/18		Scale @ A1 1 : 1250		
Building Grid Reference CADP						
Proj. Code A400PAW	Orig. A	Disc. Zone 14	Level XXXXXX	Title DRGA900-006	Rev. C	Status S3



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Legend

- Indicative Contractors Compound
Area: 2.05073 hectares
- Temporary Barge Berths & Crane Platforms
- Existing Timber Palisade
- Existing Metal Palisade
- Proposed Mesh Fence
- Proposed Construction Noise Barrier - 3 metres
- Proposed Construction Compound Timber Hoarding and Noise Barrier - 3 metres
- Application Boundary

SCALE BAR

N

D	MDS	09/01/18	For Approval
Previously issued as A400-PAW-A-13-XXX-DR-GA-948-002			
C	MDS	02/12/16	For Approval
B	MDS	10/11/16	For Planning Approval
A	MDS	25/10/16	For Planning Approval
Rev	Drn	Date	Description

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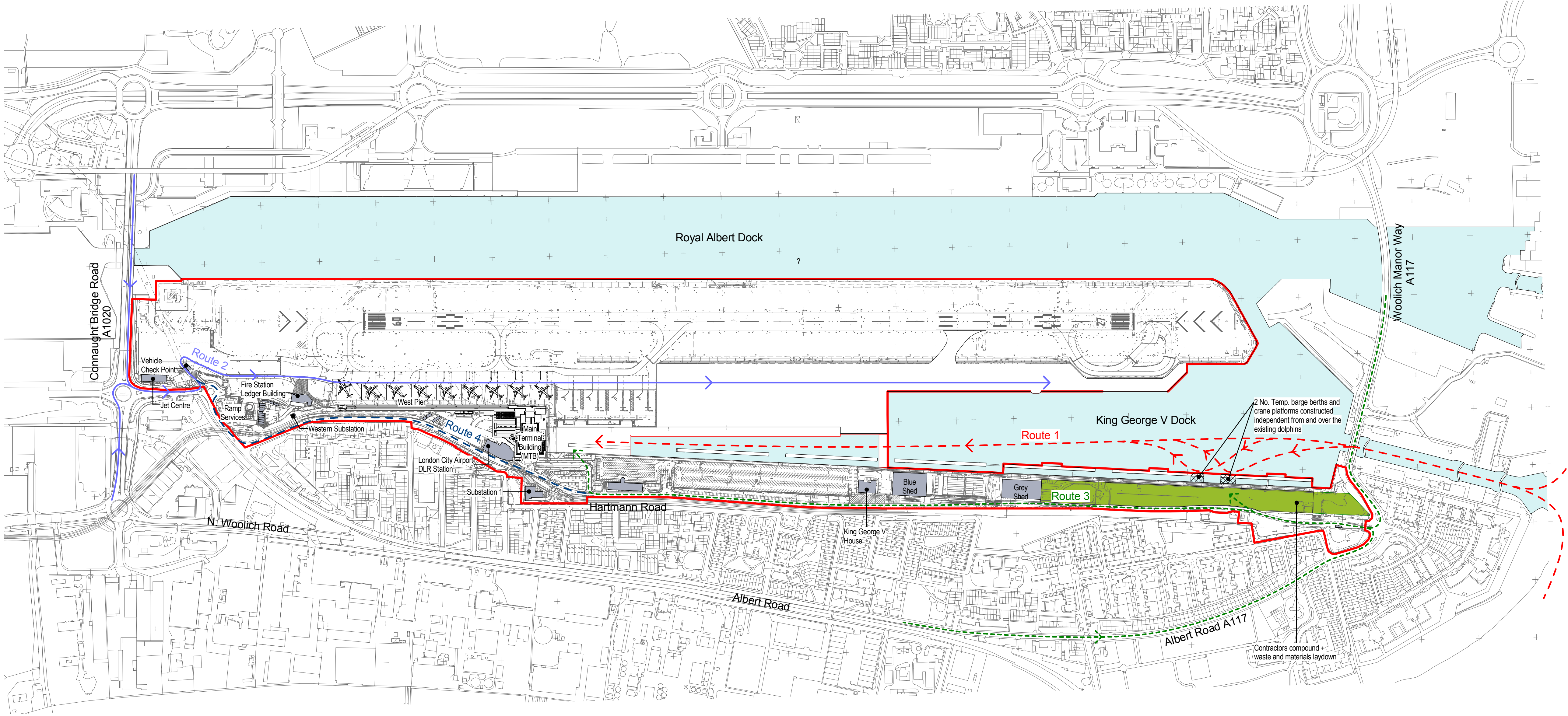
Project Name
CITY AIRPORT DEVELOPMENT PROGRAMME

Title
CADP Condition 96
Figure 2: Construction Compound & Details

Discipline Architecture		Purpose of Issue For Approval	
Drawing Originator Pascall+Watson architects		Originators Job No. 5077	
Checked By TA	Checked date 09/01/18	MS	Drawn Date 25/10/16
Approved By MN	Approval Date 09/01/18	Scale @ A1 1 : 1250	
Building Grid Reference CADP			
Proj. Code A400PAW	Orig. A	Disc. Zone 14XXXX	Level DR
Title GA	Series 900-007	Rev. D	Status S3

Appendix 2 - Construction Access Routes (for information)

Construction Access Routes A400-PAW-A-14-XXX-XX-DR-GA-900-008-B-S2



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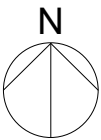
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- Base building survey information by LCY and MSA.

Legend

- Route 1 - Barge Access
- Route 2 - Airside Site Access
- Route 3 - Compound & Landside Site Access
- Route 4 - Secondary Compound & Landside Site Access
- Contractors Compound + Waste + Materials Laydown
- Temporary Barge Berths & Crane Platforms
- Existing Facilities
- Application Boundary

SCALE BAR



B	MDS	22/01/18	For Information
Previously Issued as A400-PAW-A-13-XXX-DR-GA-948-011 (Rev.1)			
1	MDS	14/02/17	For Information
Rev	Drn	Date	Description

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Client	LONDON CITY AIRPORT
Project Name	CITY AIRPORT DEVELOPMENT PROGRAMME
Title	CADP Site Wide Construction Access Routes

Discipline Architecture		Purpose of Issue For Information						
Drawing Originator Pascall+Watson architects		Originators Job No.						
Checked By MN	Checked date 22/01/2018	Drawn By MS	Drawn Date 22/01/2018					
Approved By MN	Approval Date 22/01/2018	Scale @ A1 1 : 4000						
Building Grid Reference CADP								
Proj. Code	Orig.	Disc.	Zone	Level	Title	Subtype/Orig. Series/NO.	Rev.	Status
A400PAW	A	14	XXXX	DR	GA900-008	B	S2	

1 Sitewide Schematic Layout of Contractors Facilities and Access

1 : 4000